

HSR Redding, CA, to Dunsmuir



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TRANSPORT SOLUTIONS

Designing Corridors for High-Speed Rails

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The
High-Speed
Rail Corridor
between
Redding, CA
and
Dunsmuir

HSR Dunsmuir Section_001

- Miles from Redding to West Weed, on ground 2.17 mi, on flyovers 5.73 mi, in tunnels 49.04 mi, a total of 56.73 mi. Tunnels shorter than 4.3 miles may be double-track conventionally excavated; all the longer tunnels will use tunnel boring machines (TBM) and is in twin bores. The twin-bore tunnels will have cross-cuts to allow emergency escapes.
- This section is mostly on superstructures. The topography does not allow any different version. The rail grades are 1.3% or less, and the curve radiuses are above 20,000 ft, except at Dunsmuir.
- All trains will stop at the Dunsmuir Station for a brief inspection before the next journey. The Dunsmuir curve radius is 530' or 10.81°
- However, this expensive-to-build corridor will pay for itself due to shortening the existing rail route, the large energy savings, the reduction of maintenance costs, and the high-speed throughput for trains.

Legend



CHSR Station in Tunnel



CHSR Station on Flyovers

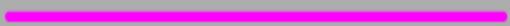


CHSR Station in on Ground

 On ground

 Cuts

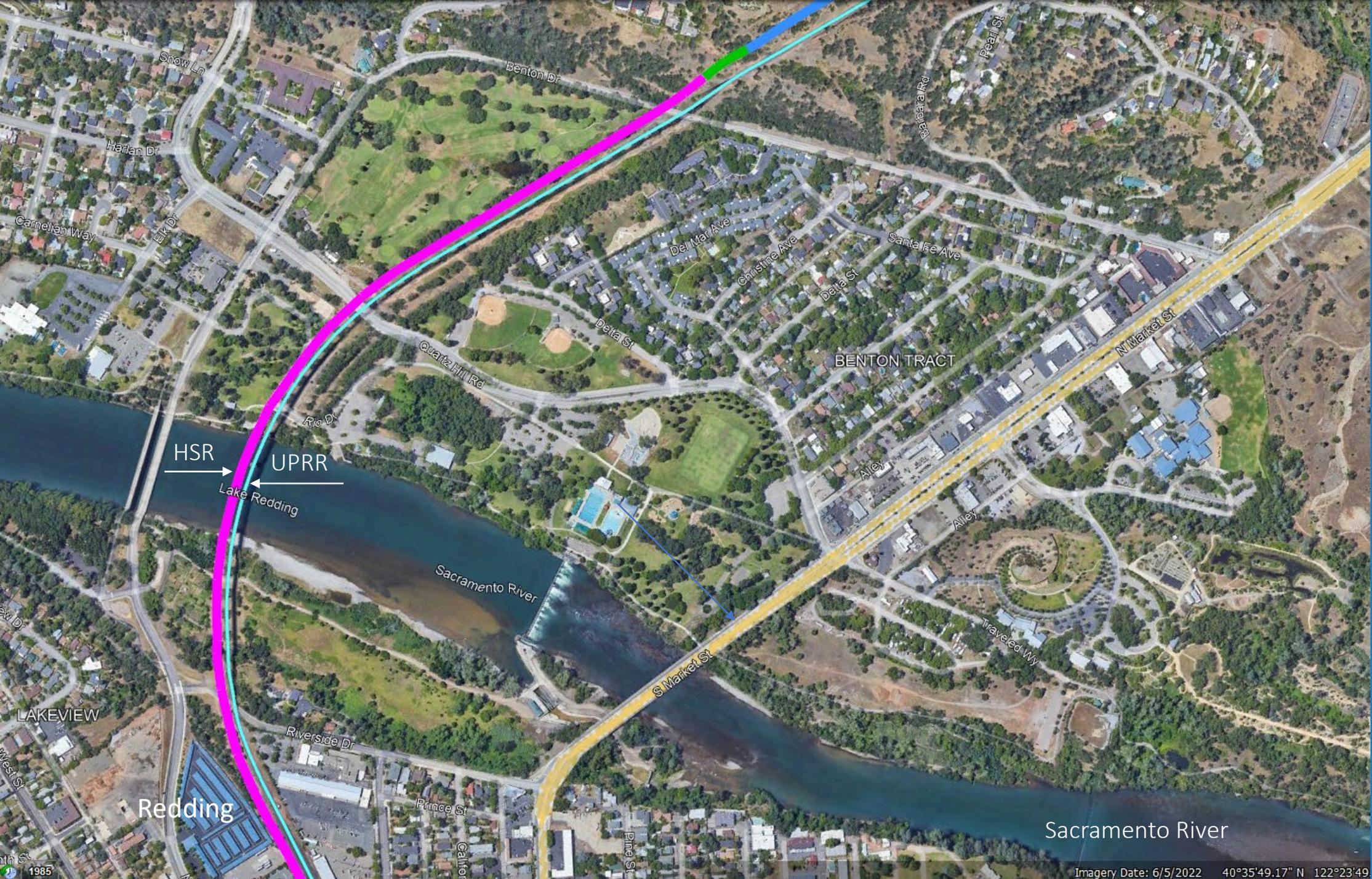
 Fills

 Flyovers

 Tunnels

 Existing Freight Railroads, other than BNSF and UP RR

 Existing Freight Railroads, and Amtrak



HSR North of Redding
Crossing the Sacramento River

The HSR runs parallel to the UPRR

HSR → UPRR ←

Lake Redding

Sacramento River

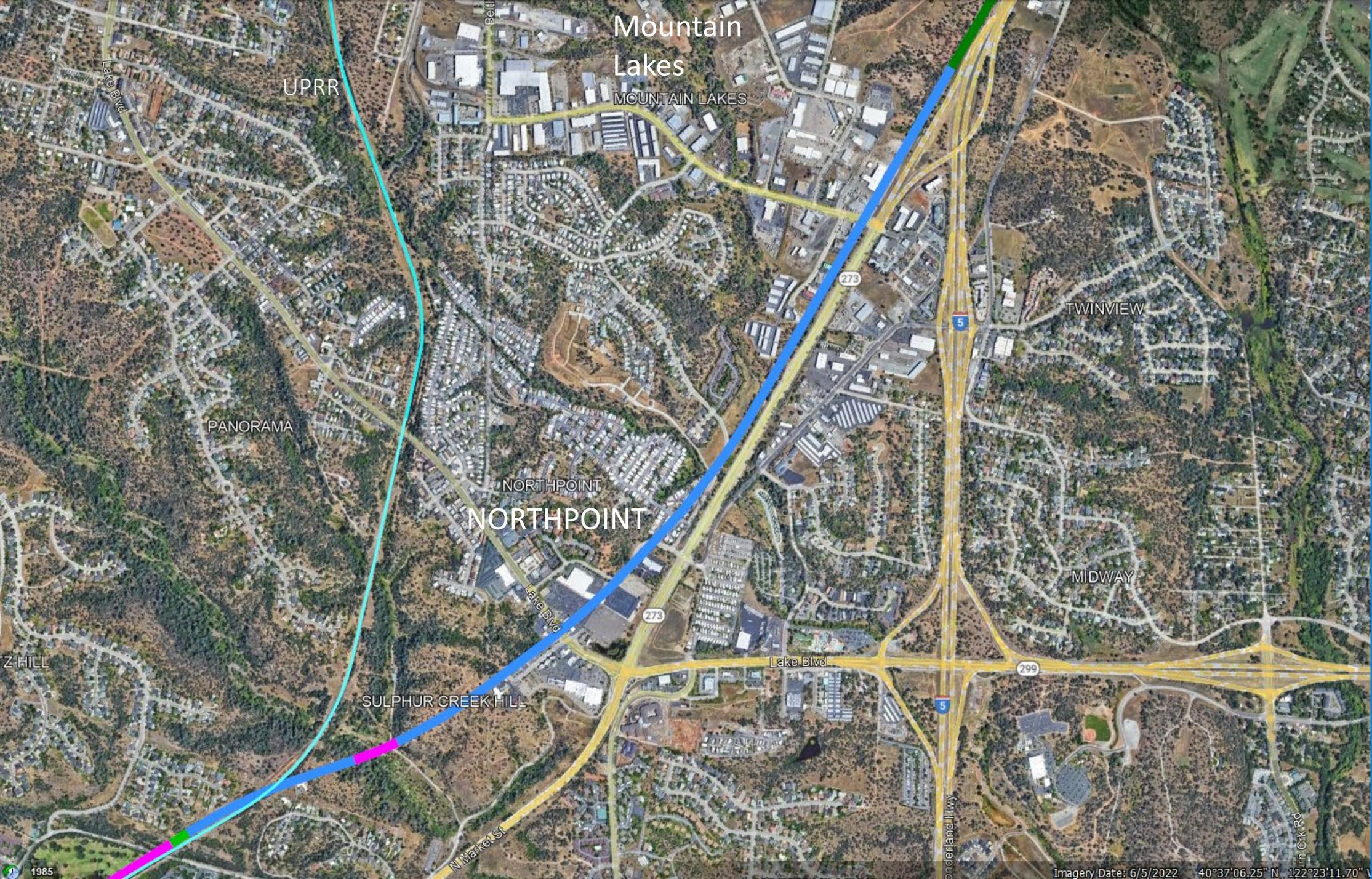
S Market St

N Market St

BENTON TRACT

Redding

Sacramento River



Mountain Lakes

UPRR

MOUNTAIN LAKES

PANORAMA

NORTHPOINT

NORTHPOINT

TWINVIEW

MIDWAY

SULPHUR CREEK HILL

Lake Blvd

299

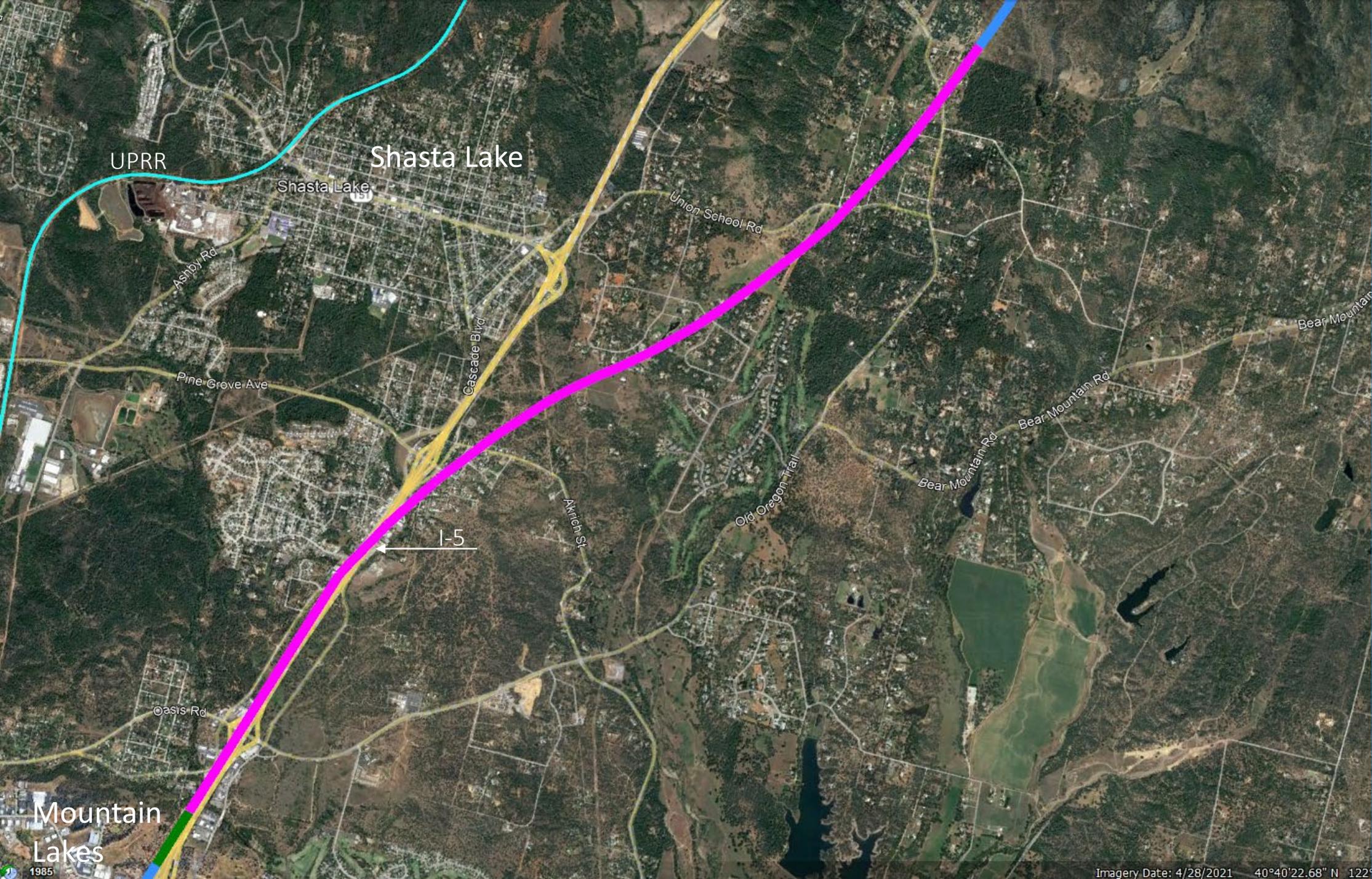
N Market St

onderland Hwy

in Crk Rd

HSR at
NORTHPOINT
and
Mountain
Lakes

The Union
Pacific freight
rail corridor is
to the west of
the HSR.

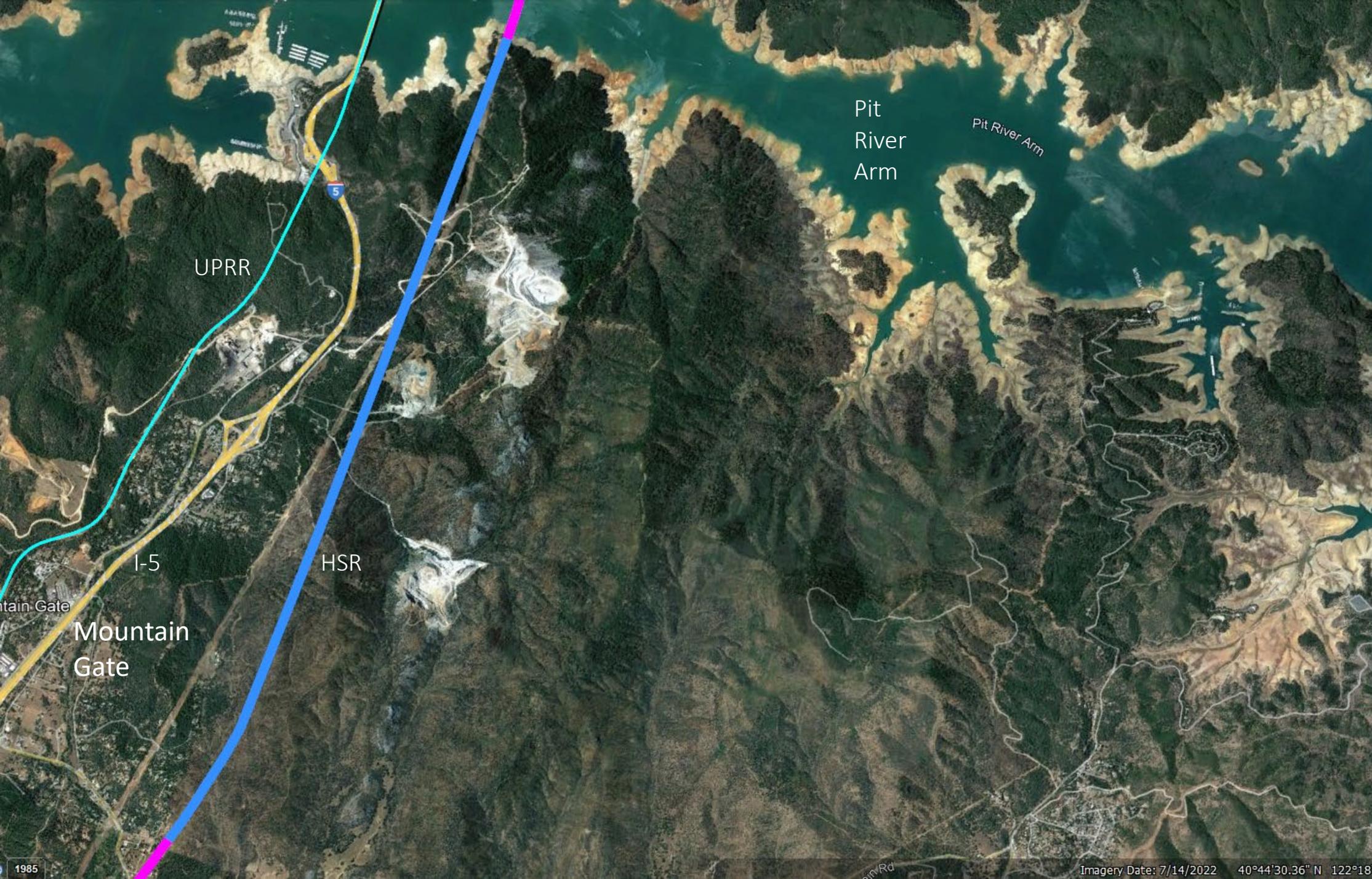


HSR between Mountain Lakes and Northeast of Shasta Lake

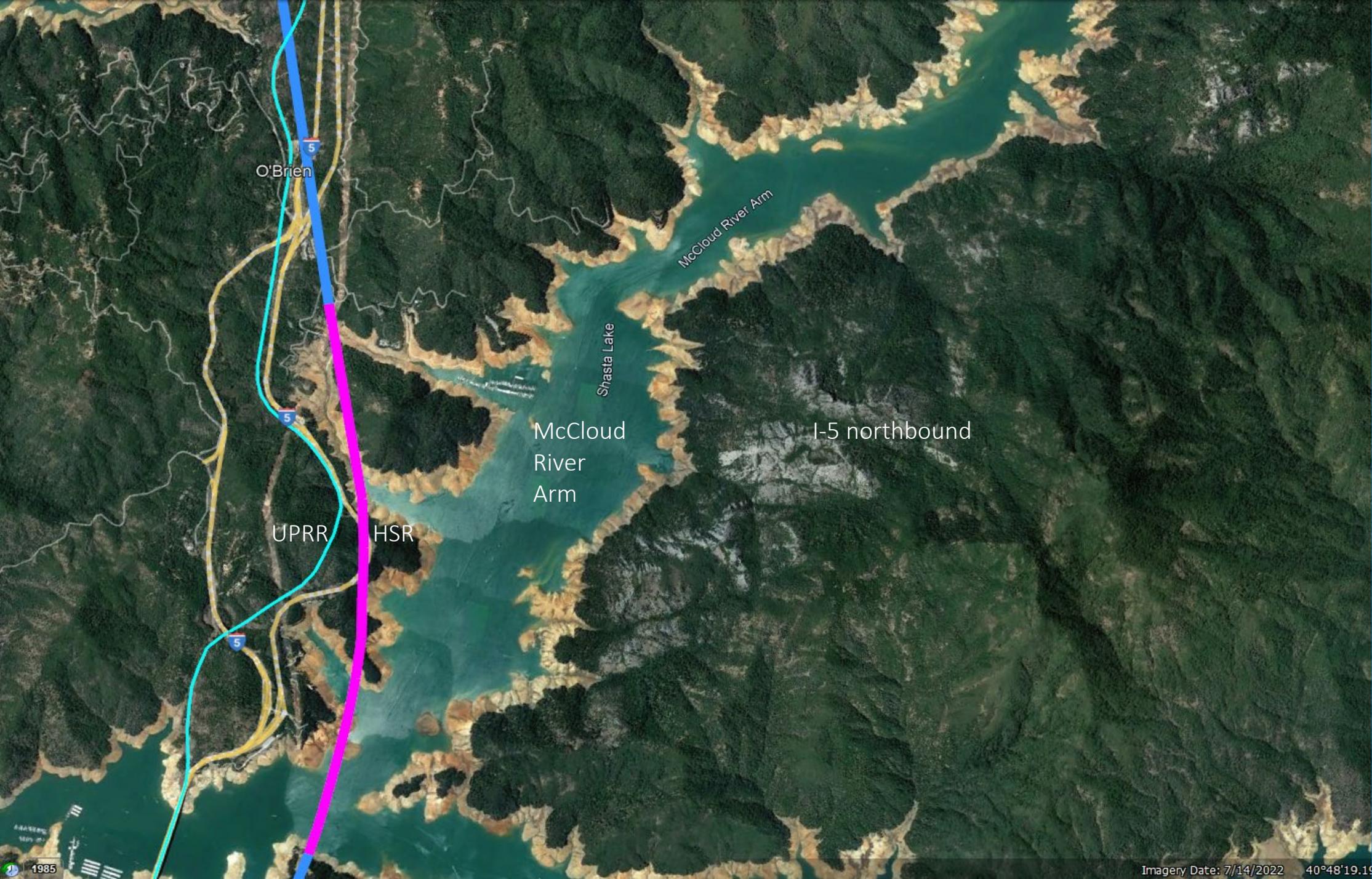
The HSR will fly over the I-5 and go through a lesser dense built-up area to connect with the tunnel.

Mountain Lakes

1985



HSR between
NE Shasta
Lake and
Shasta
Reservoir Pit
River Arm



O'Brien

McCloud River Arm

Shasta Lake

McCloud River Arm

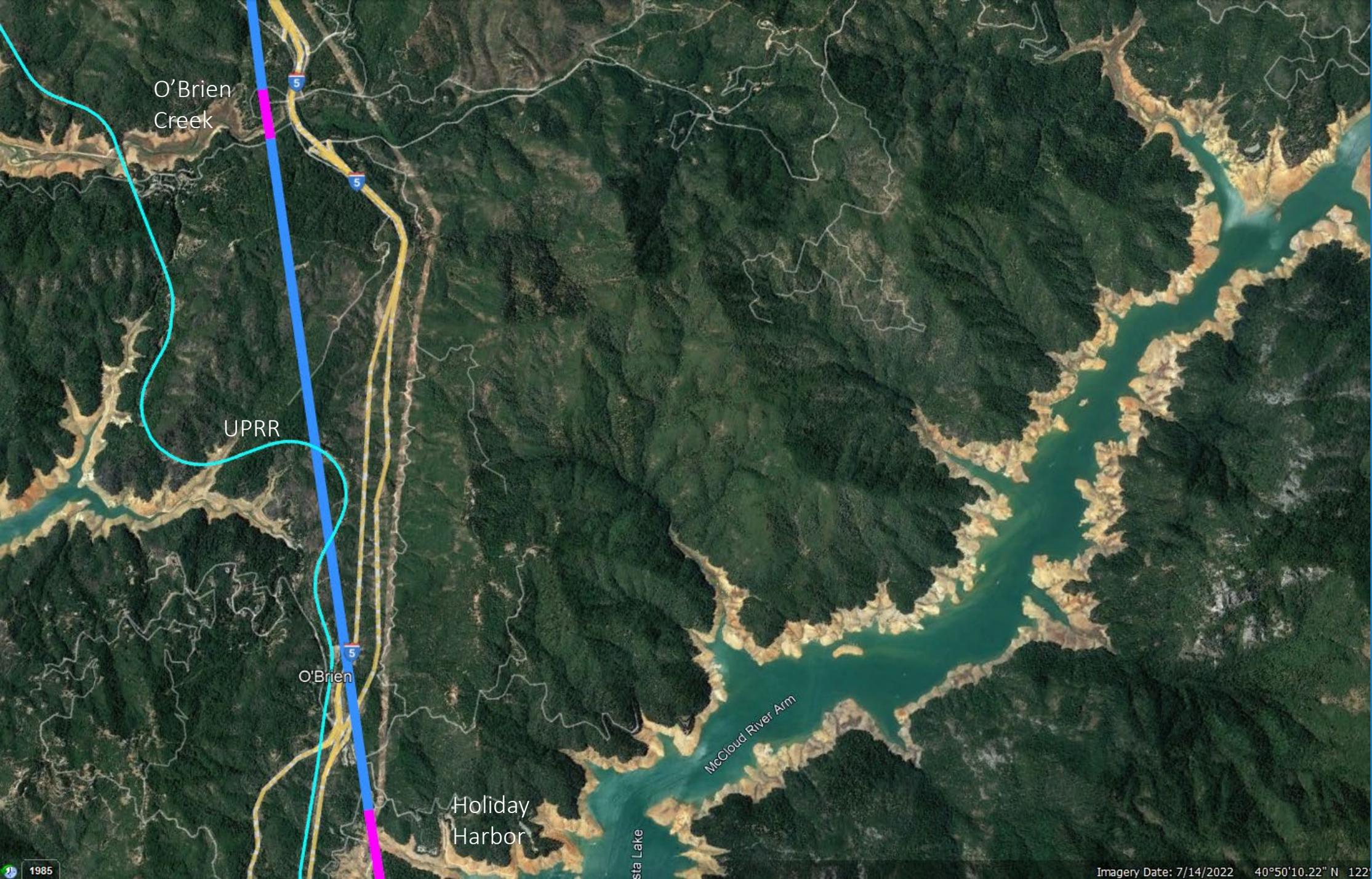
I-5 northbound

UPRR

HSR

HSR Crossing
Shasta
Reservoir
McCloud
River Arm

The HSR will fly
over the I-5
northbound
segment.



O'Brien
Creek

UPRR

O'Brien

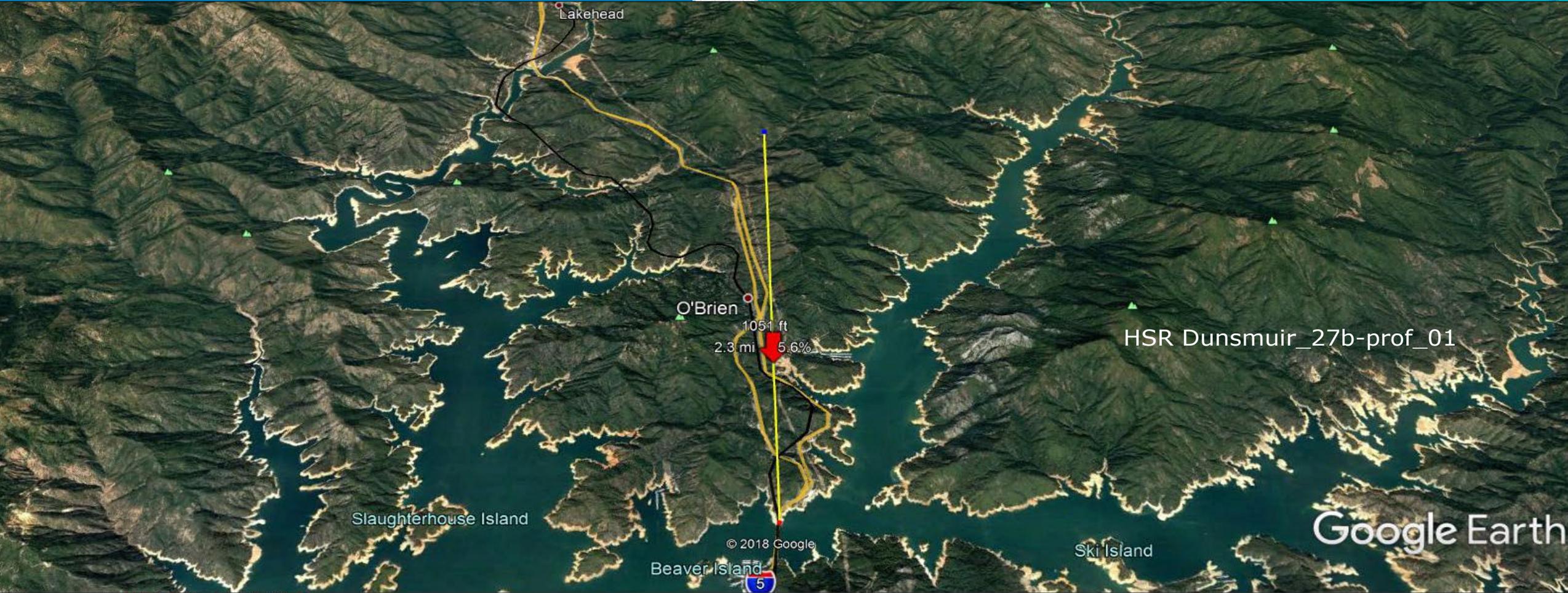
Holiday
Harbor

McCloud River Arm

ista Lake

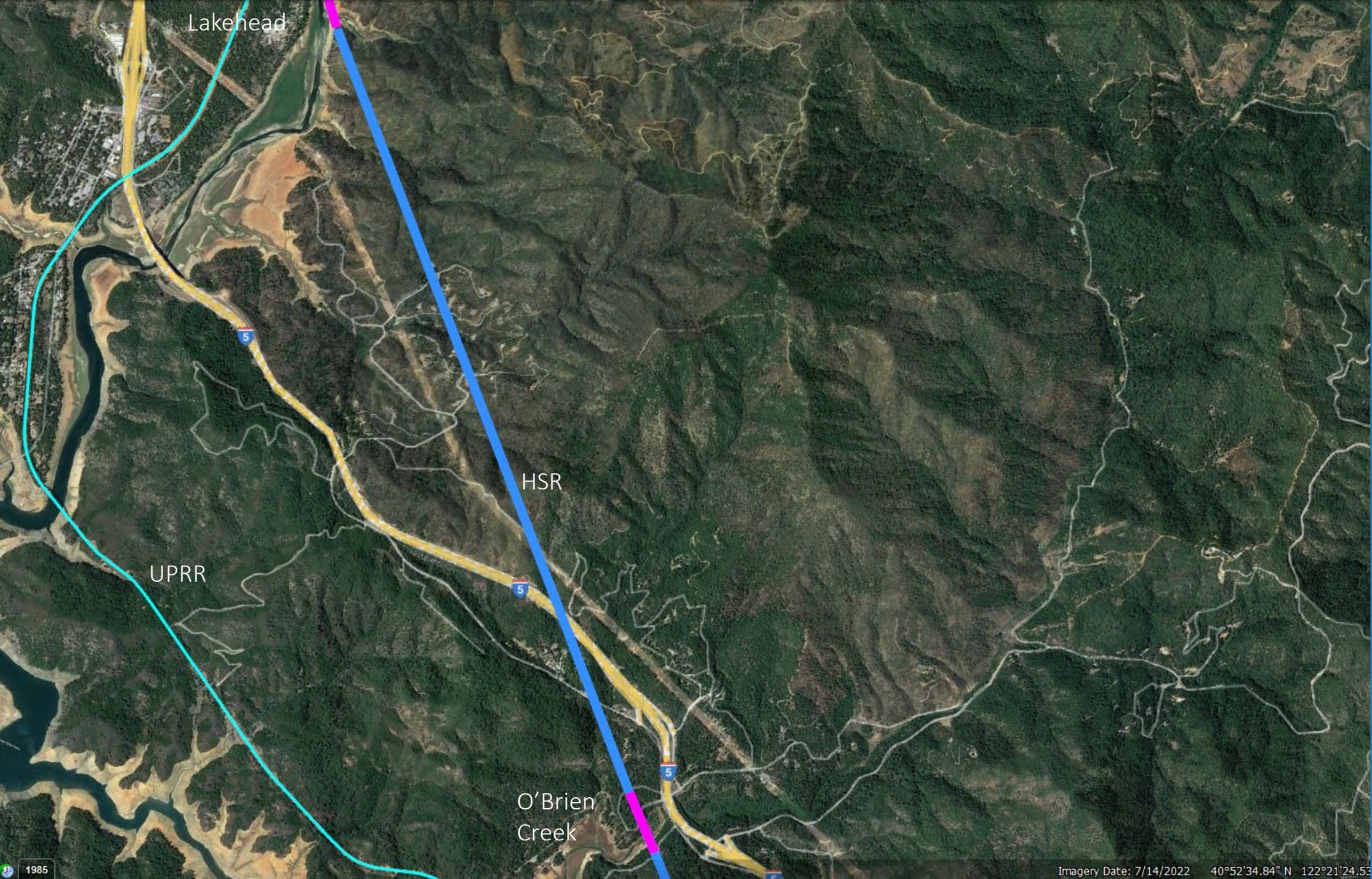
HSR between
Holiday
Harbor and
O'Brien
Creek

The CHSR
tunnel is below
the UPRR.



Graph: Min, Avg, Max Elevation: 1018, 1514, 2130 ft
 Range Totals: Distance: 6.82 mi Elev Gain/Loss: 4457 ft, -4054 ft Max Slope: 67.1%, -69.0% Avg Slope: 19.4%, -23.1%





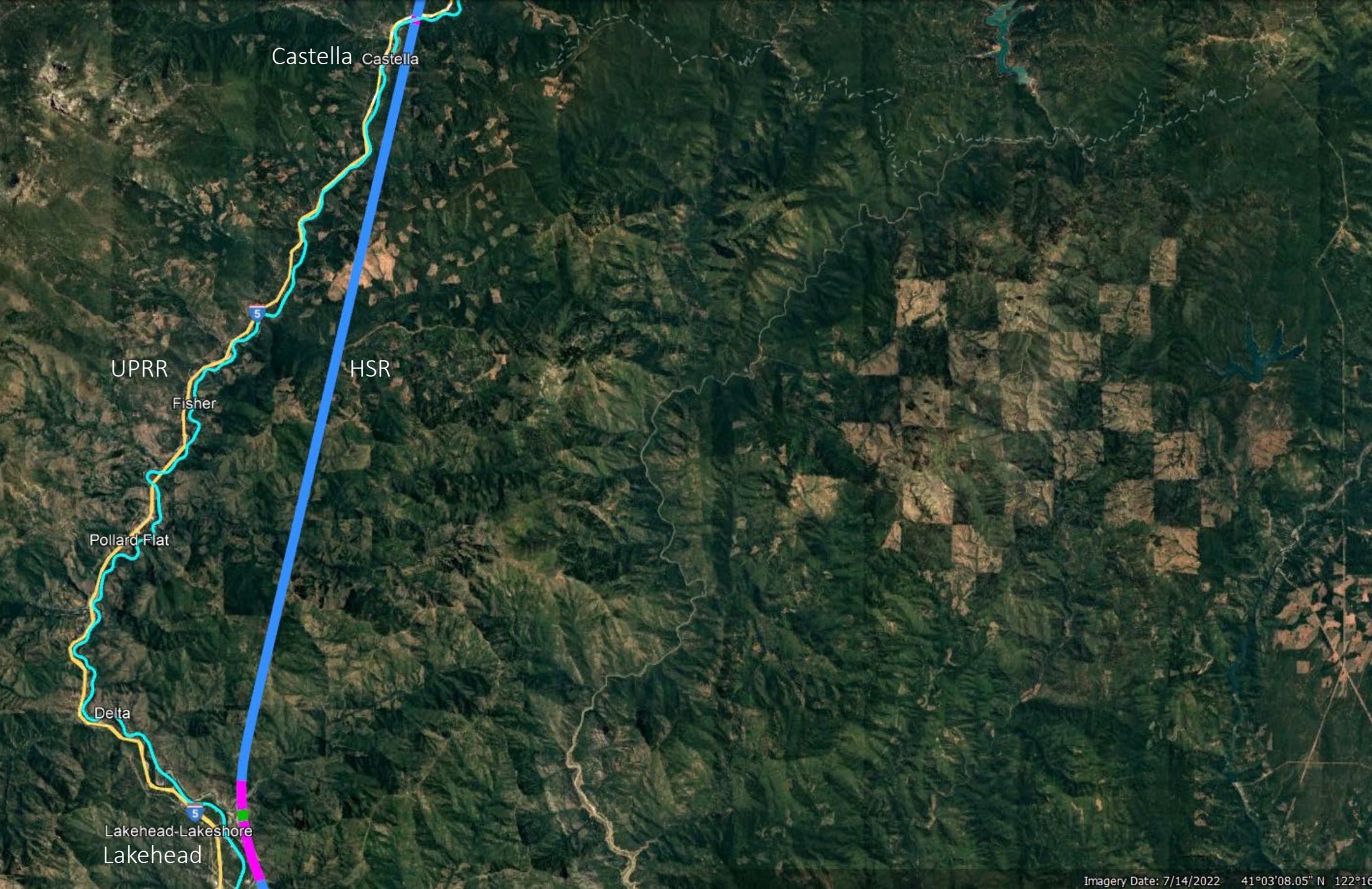
Lakehead

HSR

UPRR

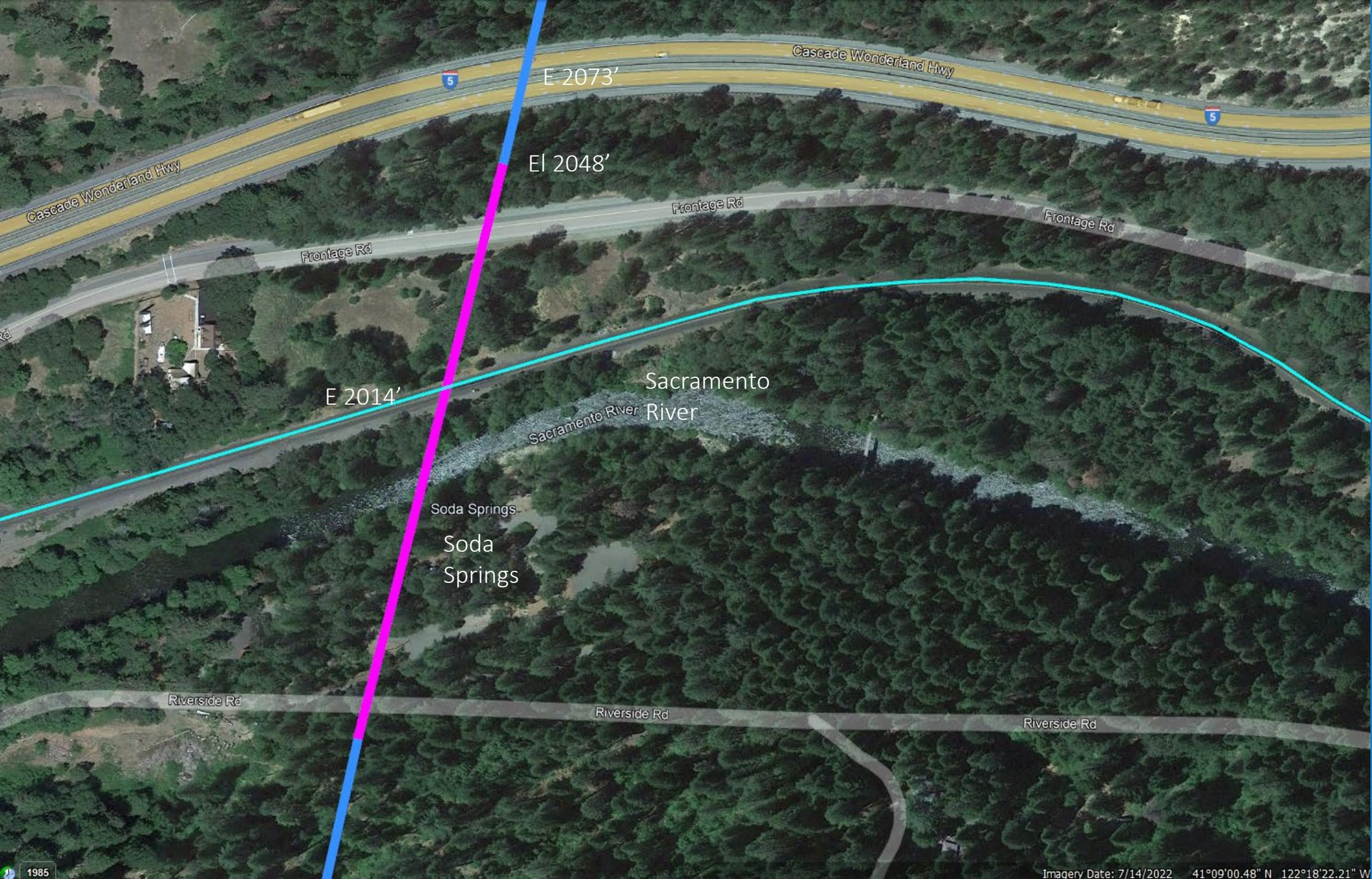
O'Brien
Creek

HSR between
O'Brien
Creek and
Lakehead



HSR between Lakehead and Castella

The HSR is in a straight tunnel; the UPRR freight tracks have many tight curves. HSR trains can not use such corridors.

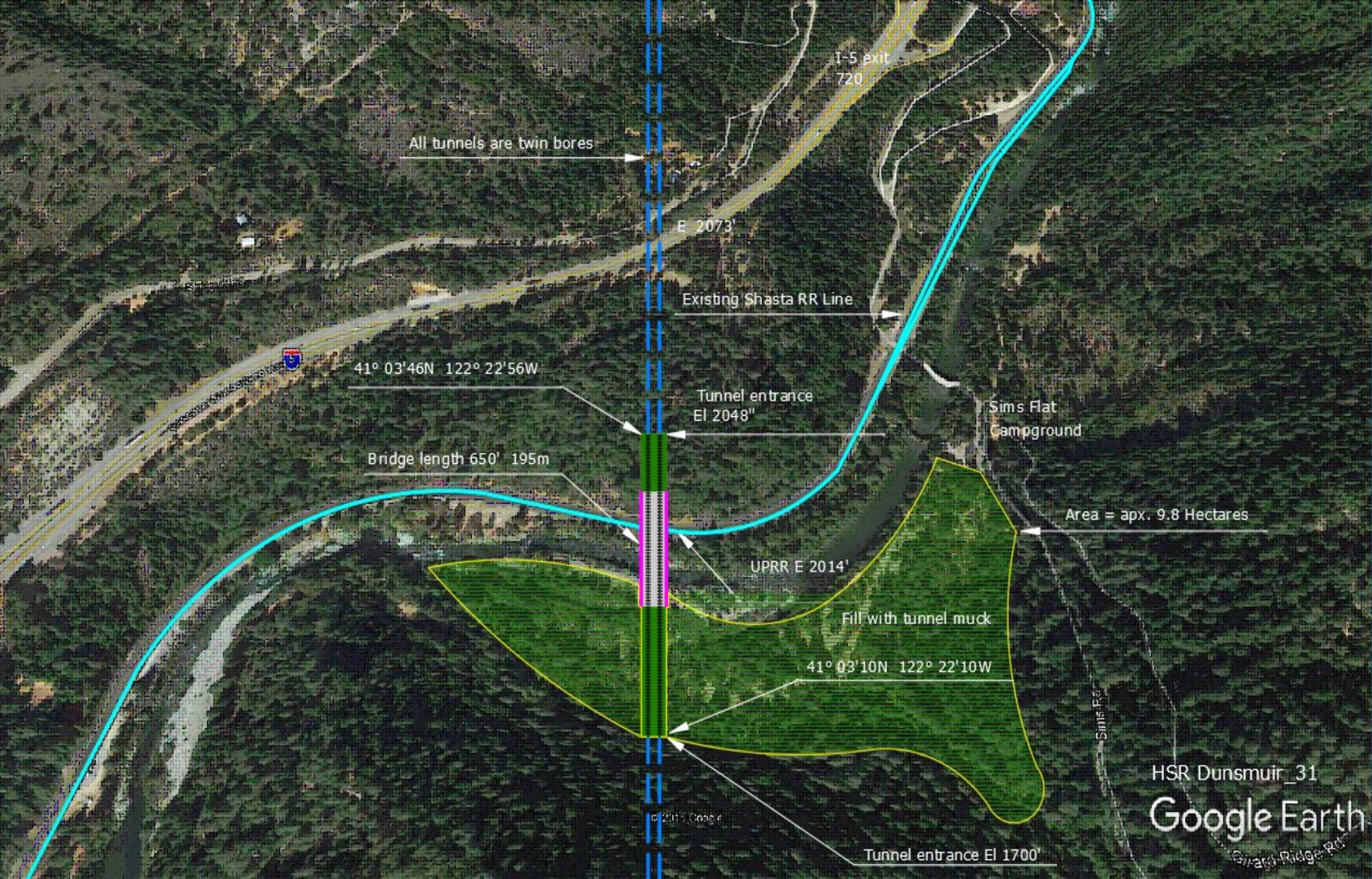


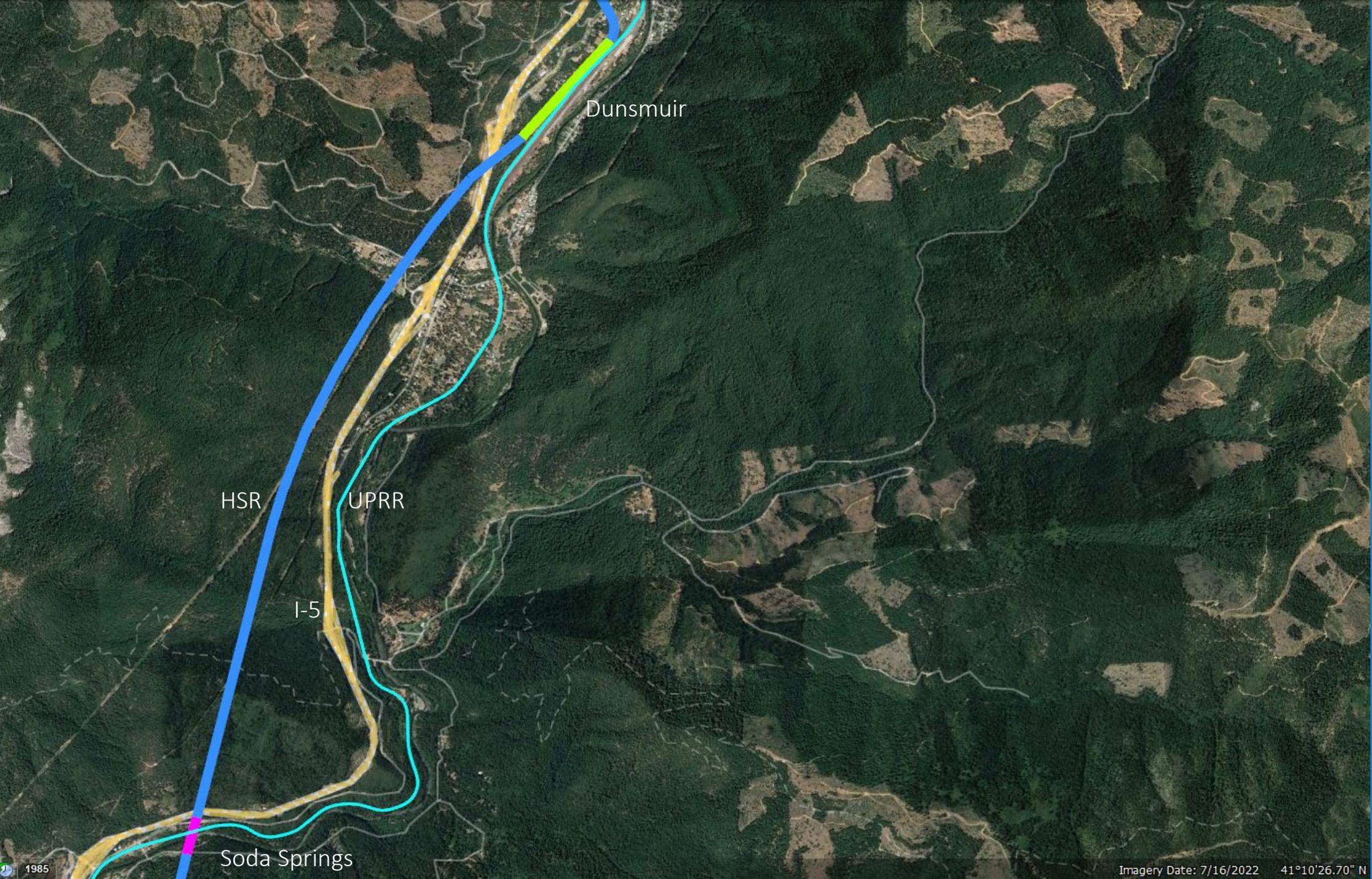
HSR at Soda Springs

Here the HSR crosses the Sacramento River. The Frontage Rd will get an underpass. The HSR is above the UPRR and passes under the I-5.

HSR
Sacramento
River
Crossing

This is an old
file created in
2016 and is
illustrative.





Dunsmuir

HSR

UPRR

I-5

Soda Springs

HSR between
Soda Springs
and
Dunsmuir

1985

Imagery Date: 7/16/2022 41°10'26.70" N



HSR Dunsmuir Station Detail

The HSR enters the tunnels on the outside of the station yard. The tunnels are in twin-bore format to allow emergency escape provisions.

The northside tunnel entrance has a tight 10.8-degree curve, but this is OK since all trains will stop at this location for a short train inspection before continuing the trips.

Train storage is possible.

HSR Dunsmuir Sta_01